



Department of **Biodiversity,
Conservation and Attractions**

Swan and Canning Rivers Management Act 2006

PART 5

DETERMINATION OF DEVELOPMENT APPLICATION

FILE NUMBER : 2017/4110
APPLICANT : City of Melville
APPLICANT'S ADDRESS : Attn: Ian Davis
Projects Coordinator
City of Melville
Locked Bag 1
BOORAGOON WA 6954
LANDOWNER : Swan River Trust (River Reserve 48327) and City of Melville
LAND DESCRIPTION : Deep Water Point Reserve, Lot 0 on Diagram 12675 and
Lot 3771 on Plan 31418 (R30732) The Esplanade, Mount
Pleasant and the Canning River, Lot 310 on Plan 47439
(Reserve 48327)
DEVELOPMENT : Replacement of a boat ramp and associated foreshore
works
VALID FORM 1 RECEIVED : 31 August 2017
DETERMINATION : **APPROVAL WITH CONDITIONS**

The application to commence development in accordance with the information received on 31 August 2017, and additional information received on 27 November 2017, is APPROVED subject to the following conditions:

1. Approval to implement this decision is valid for two (2) years from the date of the approval. If substantial on-site works have not commenced within this period, a new approval will be required before commencing or completing the development.

Prior to the commencement of works

2. The applicant shall notify the Department of Biodiversity, Conservation and Attractions in writing not less than seven (7) days prior to the commencement of works (see **Advice Note 1**).
3. Prior to the commencement of works, a Construction Environmental Management Plan shall be submitted to, and approved by the Department of Biodiversity, Conservation and Attractions (see **Advice Note 2 and 7**).
4. Prior to the commencement of works, final design plans/drawings for the new boat ramp facility, including details of the external colours, finishes and materials, shall be submitted to, and approved by the Department of Biodiversity, Conservation and Attractions on advice from the Department of Transport (see **Advice Note 3 and 7**).

5. Prior to the commencement of works, an Acid Sulphate Soils (and Dewatering) Management Plan shall be submitted to, and approved by the Department of Biodiversity, Conservation and Attractions (see **Advice Note 4 and 7**).
6. Prior to the commencement of works, a Traffic Management Plan shall be submitted to, and approved by the Department of Biodiversity, Conservation and Attractions (see **Advice Note 5 and 7**).
7. Prior to the commencement of works, a Landscape Management Plan shall be submitted to, and approved by the Department of Biodiversity, Conservation and Attractions for approval (see **Advice Note 6 and 7**).

During works

8. All works shall be implemented in accordance with the application submitted on 31 August 2017, additional information submitted on 27 November 2017, and management plans and final design drawings/plans approved under **Conditions 3-7**. The area marked as 'Carpark Upgrades' on Drawing D1369-01-01 (Rev. B) does not form part of this approval.

On completion of works

9. Upon completion of the works, all waste materials, equipment and machinery shall be removed and the site cleaned-up to the satisfaction of the Department of Biodiversity, Conservation and Attractions.

ADVICE TO APPLICANT

1. Notification of commencement of work and submission of documents can be emailed to rivers.planning@dbca.wa.gov.au.
2. The Construction Environmental Management Plan required under **Condition 3** shall describe how the proposed works will be managed to minimise environmental impacts and should address, but not be limited to;
 - scope of works, including demolition and construction methodology;
 - site access and management, including fencing requirements;
 - management of machinery and equipment, including refuelling procedure and spill response;
 - on-site storage of materials, equipment, chemicals and fuel;
 - protection of the river from inputs of debris, litter, rubbish or other deleterious material;
 - protection of vegetation (including tree roots);
 - sediment and turbidity management;
 - fauna management;
 - marine safety;
 - hours of operation and schedule of works; and
 - complaints and incidents.

The plan should address the most appropriate means of sediment and turbidity management for the proposed works. This may include the use of a silt curtain. All reasonable efforts should be taken to control plumes but where they occur, work must stop whilst the plume is addressed, and work should only recommence once the integrity of the device is restored. In the event that plumes, or adverse turbidity occur beyond the area of work, the applicant shall contact the Department of Biodiversity, Conservation and Attractions, Rivers and Estuaries Division within 2 hours on 9278 0900 or a/h 0419 192 845. Emergency spill kits should be onsite at all times and refuelling of machinery should be undertaken outside the Swan Canning Development Control Area. In case of pollution

events the Department of Biodiversity, Conservation and Attractions should be contacted immediately on 9278 0900 or a/h 0419 192 845. A site map showing the laydown area and vehicle entry/exit points should be included. Sand, soil, and any other finely grained material that is exposed or stockpiled on site shall be managed to prevent sediment mobilisation and discharge into the Canning River. The existing mature tree (including its roots) in close proximity to the scour protection should be appropriately protected during the construction works. Piling operations shall include a soft start up procedure at the commencement of each piling sequence. The contractor shall keep a constant watch for dolphins during piling operations. No pile driving shall start if dolphins are within 200m of the site and piling activity shall cease if a dolphin comes within 50m of the site and shall not recommence until any dolphin has moved away more than 200m or has not been observed for 20 minutes.

3. In relation to **Condition 4**, the Department of Transport advises that all detailed drawings/plans for the new boat ramp are to be signed and certified by an experienced maritime engineer to confirm that it has been designed in accordance with AS3962-2001 Guidelines for Design of Marinas and AS4997-2005 Guidelines for the Design of Maritime Structures. Any foreshore re-grading works adjacent to the scour protection should also be included on the plans. The selection of colours, finishes and materials should complement the character and landscape setting of the Canning River. In addition, given the river setting, the height of the navigational light should be reduced.
4. In relation to **Condition 5**, the *Acid Sulphate Soils Guideline Series for guidance on the identification, assessment and management of acid sulphate soils in Western Australia* is available from the Department of Water and Environmental Regulation website at www.dwer.wa.gov.au. If any ASS is exposed during the works, the Contaminated Sites Branch of the Department of Water and Environmental Regulation should be contacted for further advice on 1300 762 982. Any dewatering requirements will need to be addressed in the plan.
5. In relation to **Condition 6**, public access along the foreshore should be maintained, or an alternative route provided with appropriate signage, for the entirety of the works to the satisfaction of the Department of Biodiversity, Conservation and Attractions.
6. In relation to **Condition 7**, planting behind the scour protection is required to assist with improving its visual impact. The Landscaping Management Plan should include a list of plant species to be used, their location and densities and implantation schedule. Please note that all plant species are to be locally native and suited to the soil type of the area to sustain local biodiversity and due to their low fertiliser and water requirements. Details of any temporary fencing/screening to be implemented should also be provided.
7. To allow sufficient time for the Department of Biodiversity, Conservation and Attractions to consider and approve the plan and details required under **Conditions 3-7** inclusive, the documents should be submitted to the Department of Biodiversity, Conservation and Attractions no later than 30 days prior to the expected commencement date for the works.
8. The City of Melville will need to apply for, and obtain a jetty modification licence from the Department of Transport Coastal Facilities Management prior to the commencement of works.
9. In regard to marine safety, the applicant is advised to ensure that:
 - a. all in-water operations and activities must be conducted in accordance with Western Australian Marine Safety Legislation; and
 - b. all contractor vessels must display signs and shapes in accordance with the requirements of the *Prevention of Collision at Sea Regulations 1983*.

10. The Department of Transport (DoT) advises that the City of Melville, or any third party contractor it engages to carry out the works, notify DoT, Navigational Safety by email to navigational.safety@transport.wa.gov.au prior to the commencement of works to enable a Temporary Notice to Mariners outlining the scope of the works, locations, dates and navigational markings, to be issued, funded by the City of Melville or its contractor. It is advised that at least 21 days notice be provided.
11. The applicant is advised that the proposal has the potential to be affected by the 1% Annual Exceedance Probability Flood Level.
12. The applicant is advised to contact the Department of Planning, Lands and Heritage to ensure that the proposed works do not breach any section of Part IV (Protection of Aboriginal Sites) of the *Aboriginal Heritage Act 1972* (the AHA). Approval under section 18 of the AHA may be required. It is recommended that the applicant consult the recognised knowledge holders (see below) for Aboriginal Site 3536 on whether the works will impact the Canning River. Please refer to the Aboriginal Heritage Due Diligence Guidelines when planning specific development within a Heritage area.
13. A future interpretation node has been identified in the *Marli Riverpark: An Interpretation Plan for the Swan and Canning Riverpark* (Swan River Trust, April 2014) at Deep Water Point. The City of Melville is encouraged to work with the Department of Biodiversity, Conservation and Attractions to develop an interpretation node for this area.



Hon Stephen Dawson MLC
MINISTER FOR ENVIRONMENT

Date: 13/3/18

DEPARTMENT OF BIODIVERSITY, CONSERVATION AND ATTRACTIONS REPORT

PROPOSAL : Replacement of a boat ramp and associated foreshore works
COST : \$1,200,000
LOCATION : Deep Water Point Reserve, Lot 0 on Diagram 12675 and Lot 3771 on Plan 31418 (R30732) The Esplanade, Mount Pleasant and the Canning River, Lot 310 on Plan 47439 (Reserve 48327)
APPLICANT : City of Melville
LANDOWNER : Swan River Trust (River Reserve 48327) and City of Melville
LOCAL GOVERNMENT : City of Melville
MRS CLASSIFICATION : Parks and Recreation
DECISION TYPE : Part 5, *Swan and Canning Rivers Management Act 2006* – Ministerial Determination
ATTACHMENTS : 1. Location map
2. Aerial photo of subject land
3. Plans
4. Indicative plan of the boat ramp and finger jetty at mean sea level
RECOMMENDATION : **APPROVAL WITH CONDITIONS**

REPORT

1.0 INTRODUCTION

- 1.1 The Department of Biodiversity, Conservation and Attractions (DBCA) has received an application from the City of Melville (the City) proposing to replace the existing boat ramp and add a new finger jetty at Deep Water Point, Lot 0 on Diagram 12675 and Lot 3771 on Plan 31418 (R30732) (see **Attachment 1 and 2**) The Esplanade, Mount Pleasant and the Canning River (Reserve 48327). The subject land and River reserve are reserved for 'Parks and Recreation' and 'Waterways' respectively under the *Metropolitan Region Scheme*.
- 1.2 Lot 0 on Diagram 12675 and Lot 3771 on Plan 31418 (Reserve 30732) are vested with the City of Melville and the Canning River is vested with the Swan River Trust (the Trust).
- 1.3 The proposed development is to occur on lots located completely within the Swan Canning Development Control Area (DCA) and therefore requires an approval from

the Minister for Environment in accordance with Part 5 of the *Swan and Canning Rivers Management Act 2006* (SCRM Act).

- 1.4 The Director General of DBCA has prepared this report in accordance with Section 76 of the SCRM Act.

2.0 CONSULTATION

Department of Planning, Lands and Heritage

- 2.1 The Department of Planning, Lands and Heritage (DPLH) has reviewed the proposal and advised that a section of the proposed works will be located within a Registered Site of Aboriginal Significance (ID 3536 Swan River).
- 2.2 Accordingly, the applicant may need to submit a section 18 application under the *Aboriginal Heritage Act 1972* (AHA).
- 2.3 DPLH further advised that the City should consult the recognised knowledge holders for Aboriginal Site 3536 (Swan River) in regard to the potential impacts the proposed development may have on the river prior to the submission of the section 18 application. The City should also refer to the *Aboriginal Heritage Due Diligence Guidelines* (Department of Aboriginal Affairs, 2013).

Department of Water and Environmental Regulation

- 2.4 The Department of Water and Environmental Regulation (DWER) advises that the proposed area is affected during major storm events, with the 1 in 100 annual exceedance probability (AEP) flood level being 1.3m AHD (approx.), increasing to 2.2 m AHD (approx.) should sea level rise by 0.9m in 2110.
- 2.5 DWER has assessed the proposal and notes that the boat ramp facility is a non-habitable structure and unlikely to detrimentally effect the existing flood regime, and therefore is considered acceptable with regard to major flooding.

Department of Transport – Maritime Planning

- 2.6 The Department of Transport (DoT) Maritime Planning branch has reviewed the proposal and advises that it has no in-principle objection to the proposed development subject to the following recommendations:
- The City will need to apply for, and obtain a jetty modification licence from DoT, Coastal Facilities Management branch; and
 - All drawings/plans for the new boat ramp and jetty will need to be signed and certified by an experienced maritime engineer to confirm that they have been designed in accordance with AS3962-2001 Guidelines for Design of Marinas and AS4997-2005 Guidelines for the Design of Maritime Structures.

Department of Transport – Navigational Safety

- 2.7 DoT Navigational Safety has reviewed the proposal and advises that it has no objection to the proposed development subject to the following recommendations:
- All in-water operations are conducted in accordance with the relevant maritime legislation; and

- Prior to the commencement of works, a Temporary Notice to Mariners (TNTM) must be issued by DoT Navigational Safety outlining the scope of works, location, dates and navigational markings. The applicant is advised to provide notice at least 21 days prior to works commencing, with notification to be emailed to navigational.safety@transport.wa.gov.au.
- 2.8 DoT Navigational Safety has also advised that the adjacent Canning River Water Ski area will need to be amended in the Government Gazette to provide a 50m buffer from the end of the boat ramp facility.

3.0 PUBLIC SUBMISSIONS

- 3.1 In accordance with the requirements of Part 5 of the SCRM Act, a copy of the draft report and proposed recommendations was provided to the applicant and the relevant agencies. A copy was also published on DBCA's website for a period of three weeks between 3 January 2018 and 26 January 2018.
- 3.2 Two submissions were received:
- One submission was from DWER advising that it has no additional comments to provide;
 - One submission was from DoT requesting an amendment to Section 2.7 (Consultation – Department of Transport) to include the advice that the applicant is to provide at least 21 days notice (via email to navigational.safety@transport.wa.gov.au) to enable a TNTM and gazettal notice to be published.

4.0 RELEVANT POLICIES AND PLANS

- ◆ State Planning Policy 2.10 – *Swan-Canning River System (SPP 2.10)*
- ◆ Corporate Policy Statement No. 42 – *Planning for Land Use, Development and Permitting Affecting the Swan Canning Development Control Area (Policy 42)*
- ◆ Corporate Policy Statement No. 44 – *Planning for Jetties in the Swan Canning Development Control Area (Policy 44)*

5.0 ENVIRONMENTAL AND PLANNING CONSIDERATIONS

- ◆ Environmental Protection
- ◆ Jetty Structures
- ◆ Visual Amenity
- ◆ Heritage Protection
- ◆ Flood Risk and Sea Level Rise
- ◆ Public Access and Safety

6.0 BACKGROUND

- 6.1 The existing concrete boat ramp at Deep Water Point is located on the western bank of the Canning River in Mount Pleasant and adjacent to the Canning River Water Ski area. The City has advised that the boat ramp facility was originally constructed in 1965. In 2013, the City developed a Draft Concept Plan for Deep Water Point to

upgrade existing facilities (e.g. cafe, new jetty, playground, boat ramp and parking) and to address erosion issues.

- 6.2 Public consultation was sought as part of the Concept Plan process, with a majority of the feedback received by the City being in relation to improving parking and boating facilities. The Deep Water Point boat ramp is a heavily used boat launching facility, and it has been identified that during strong wind and wave conditions, the launching and retrieval of boats can be difficult.
- 6.3 As part of the Deep Water Point upgrade works, DBCA granted a permit to the City to replace a wooden jetty in 2013 (SRT5067), with the construction works completed in 2014. A Part 5 Development Approval (2016/3929) was also granted in 2016 to Dome Coffees Australia to redevelop the cafe facility (and associated toilet block), with the construction of the new cafe currently in progress. The City was also issued a permit earlier this year to construct three rock headlands (and beach regrading) along the riverbank to address erosion control (2016/3022), with the works scheduled for 2018.
- 6.4 The current application is for the replacement of the existing boat ramp, and includes the following (see **Attachment 3**):
- Demolition and removal of the existing concrete boat ramp, turning area and scour protection;
 - Construction of two boat ramp lanes (approx. 27m long and 4.5m wide per lane) consisting of pre-cast concrete panels placed over an aggregate concrete layer (min. 0.18m thick), limestone bedding layer (min. 0.6m thick) and geotextile layer. Limestone rock pitching will also be installed between the two lanes;
 - Construction of a concrete abutment and 24.5m long central finger jetty consisting of a fibre plastic reinforced deck, steel framework and 6 steel piles (6-8m long) with protective wrapping (Denso SeaShield system and DensoRockmesh) and 26 timber chafers. A 4.8m AHD high navigation light will be installed at the end of the finger jetty; and
 - Construction of scour protection on either side of the boat ramp consisting of limestone armour rocks (0.3-1m sized rocks) ranging from two layers to a single layer as it extends into the river, a limestone bedding layer (0.6m thickness) and geotextile fabric. The crest height of the rock revetment will range from 0.5m AHD (equivalent to HAT) to below - 1.00 mAHD (and below LAT). The crest width will be 2.4m with the first three rocks adjacent to the ramp to be grouted.
- 6.5 It is noted that at mean sea level (-0.1m AHD), the finger jetty and a portion of the boat ramp and scour protection will be visible from the water (see **Attachment 4**).
- 6.6 The City has engaged engineers MP Rogers & Associates to develop detailed designs and technical specifications for the new boat ramp, and it is anticipated that the two new ramp lanes and central finger jetty will improve the useability of the facility.
- 6.7 The design drawings for the new boat ramp facility indicate that the existing car park will be upgraded, however the City has advised that the car park upgrade works do not form part of this application.
- 6.8 The City was granted \$700,00 in funding from DoT through the Recreational Boating Facility Scheme for this project.

7.0 DISCUSSION

Environmental Protection

- 7.1 Policy 42 aims to ensure that land use and development on and adjacent to the river system maintains and enhances the quality and amenity of the river environment.
- 7.2 The proposed works will include ground excavation, concrete/grouting works, rock placement and jetty piling. The construction works have the potential to impact on the foreshore and waterway, including disturbance to the riverbed, water quality reduction, fauna and vegetation disturbance and disruption to public access and safety.
- 7.3 The technical specifications for the project state that the awarded contractor will be required to prepare a Construction and Environmental Management Plan (CEMP). The design drawings indicate that one of the existing mature trees is located in close proximity to the scour protection (south of the boat ramp), with a note stating that the trees will be protected. However, the demolition section of the technical specification state that tree roots within the clearing area will be removed.
- 7.4 To ensure that all potential environmental impacts are identified and managed appropriately, it is recommended that the CEMP be endorsed by DBCA. The CEMP will be required to include details of vegetation protection (including any tree roots within the demolition/clearing area), demolition and construction methodology, protection of water quality, machinery and equipment management (including refuelling), site access and management, public access and safety, waste management and spill response, and potential noise and vibration impacts to fauna.

Acid Sulphate Soils

- 7.5 The proposed works are to be undertaken within a high to moderate acid sulphate soils (ASS) disturbance risk area within 3m of the natural surface. A preliminary ASS assessment was completed as part of the geotechnical investigation, with the findings indicating that ASS may be present within the site.
- 7.6 The technical specifications state that an ASS Management Plan will be prepared by the contractor, and if the dewatering is required, the plan will be expanded to an ASS and Dewatering Management Plan. It is recommended that this plan be endorsed by DBCA.

Boat Ramp Design

- 7.7 Policy 44 recognises that appropriately designed and located public boating facilities such as jetties and boat ramps, form an integral part of the river system setting and are an important recreational and tourist resource.
- 7.8 SPP 2.10 describes Deep Water Point as a popular recreational node that is used extensively for aquatic based activities such as boating, rowing and water skiing. The Draft Concept Plan for Deep Water Point identified issues with the existing boat ramp, and following community consultation, recommended upgrades to the boat ramp to improve safety and useability for recreational boat users.

- 7.9 Policy 44 seeks to ensure that the location of boating infrastructure does not interfere with vessel navigation. DoT, Navigational Safety advised that the Canning River Water Ski area will need to be amended in the Government Gazette to provide a 50m buffer to the new boat ramp facility. The design drawings and technical specifications also indicate that a navigation light, to continuously beam a blue light which will be visible to 2 nautical miles (3.7km), will be installed on the riverside end of the jetty. However, given the river environment, consideration should be given to reduce the height of the navigation light as part of the final design drawings/plans.
- 7.10 During construction works, a safe navigation channel should be maintained by the contractor. DoT Navigational Safety advised that all on-water operations and activities will need to be conducted in accordance with relevant marine legislation, with a Temporary Notice to Mariners to be organised. These requirements will need to be addressed in the CEMP.
- 7.11 Policy 44 also seeks to ensure that the boat ramp design is safe for public use. The design drawings indicate that the boat ramp has been designed for 8m long vessels, with the facility to have a 25-year design life. No dredging requirements are proposed in the technical specifications or indicated on the design drawings.
- 7.12 Jetty structures (including boat ramps) must comply with the relevant Australian Standards and be certified by a qualified and practising marine engineer, as recommended by DoT Navigational Safety. These requirements can be included as a condition of approval through the submission of final design plans/drawings.
- 7.13 The technical specifications state that the scour protection for the new boat ramp will tie-in with the already approved future headlands and constructed beaches to the north of the project area (2016/3022). Any requirement to re-grade the foreshore to the south of the new boat ramp should also be addressed in the final design plans/drawings.
- 7.14 The City will be responsible for the ongoing maintenance of the new boat ramp, jetty and scour protection.

Visual Amenity

- 7.15 SPP 2.10 states that proposals for development should be considered in the context of the river setting and the characteristics of the development site, particularly in regard to the building height, bulk, scale, form, orientation and location. Policy 42 states that the design, materials and colour scheme of development adjacent to the river and foreshores should complement and protect the character and landscape setting of the river.
- 7.16 The construction materials proposed for the new boat ramp launching facility will consist of limestone rocks for the scour protection, steel framework and fibre reinforced plastic decking for the central finger jetty and precast concrete panels, concrete abutment and limestone pitching for the two boat ramp lanes.
- 7.17 Deep Water Point is a relatively modified environment, and the proposed boat ramp structure will not have a significant visual impact on the surrounding landscape and riverine environment.

- 7.18 However, given the larger footprint of the new boat ramp facility, it is recommended that planting be undertaken behind the proposed scour protection on the northern side of the boat ramp to assist with softening the visual impact of the hard structures. In addition, endorsement of the final colours and external finishes will also be required as a condition of approval.

Heritage

- 7.19 Policy 42 states that places of cultural and heritage significance, both Aboriginal and European, and of natural heritage are to be conserved.
- 7.20 The proposal is within the boundaries of an Aboriginal site DAA 3536 (Swan River).
- 7.21 DPLH advised that as the proposed works may impact upon DAA 3536 an approval under section 18 of the *Aboriginal Heritage Act 1972* is likely to be required.

Public Access

- 7.22 SPP 2.10 states that public access to the river and foreshore should be maintained while protecting the river. It is important that development near the river does not impede or restrict public access.
- 7.23 Deep Water Point Reserve is a popular recreational area used by both recreational boats and pedestrians. The technical specifications state that a Pedestrian, Cyclist & Traffic Management Plan (TMP) will be prepared by the contractor to ensure that the construction works do not adversely impact on public access through the site. It is recommended that this plan be endorsed by DBCA.
- 7.24 Public access along the foreshore pathway during the construction works should be maintained wherever possible and prevented only for safety reasons. This can be addressed as part of the TMP.
- 7.25 A future interpretation node has been identified in the *Marli Riverpark: An Interpretation Plan for the Swan and Canning Riverpark* (Swan River Trust, April 2014) at Deep Water Point.
- 7.26 Once completed, the proposed boat ramp will improve and facilitate public access to the river.

Flood Prone Land

- 7.27 Policy 42 states that development within the floodway that will significantly affect the normal flow of the Swan and Canning rivers during floods and/or will lead to the degradation of the natural features of the river system should not be supported.
- 7.28 DWER advised that the site will be affected by flooding, with the boat ramp and finger jetty (0.85m AHD) being below the 1% AEP flood level, which is estimated to be 1.3m AHD. This information can be provided as advice to the applicant noting that the design life for the facility is 25 years.

8.0 SWAN RIVER TRUST ADVICE

- 8.1 In accordance with section 75(3A) of the SCR Act, the Trust considered the draft report on 7 December 2017, where the Trust resolved to support the report and recommendations.

9.0 CONCLUSION

- 9.1 The proposed replacement of the boat ramp at Deep Water Point is expected to improve the public use and enjoyment of the river for aquatic based activities. The design of the new boat ramp appears to be appropriate for the site and its intended use and its visual impact will be improved through vegetation and the use of colours and materials that will complement the river environment.
- 9.2 Potential environmental impacts from the works can be addressed through the preparation and approval of a CEMP required as a condition of approval.
- 9.3 For these reasons, the proposal is recommended for approval subject to conditions and advice.

10.0 RECOMMENDATION – APPROVAL WITH CONDITIONS

That the Director General of the Department of Biodiversity, Conservation and Attractions advises the Minister for Environment that the City of Melville's proposal to replace the existing boat ramp (and associated scour protection) at Deep Water Point Reserve, Lot 0 on Diagram 12675 and Lot 3771 on Plan 31418 (R30732) The Esplanade, Mount Pleasant and the Canning River, Lot 310 on Plan 47439 (Reserve 48327), as described in the application received on 31 August 2017 and additional information received on 27 November 2017, be approved subject to the following conditions:

1. Approval to implement this decision is valid for two (2) years from the date of the approval. If substantial on-site works have not commenced within this period, a new approval will be required before commencing or completing the development.

Prior to the commencement of works

2. The applicant shall notify the Department of Biodiversity, Conservation and Attractions in writing not less than seven (7) days prior to the commencement of works (see **Advice Note 1**).
3. Prior to the commencement of works, a Construction Environmental Management Plan shall be submitted to, and approved by the Department of Biodiversity, Conservation and Attractions (see **Advice Note 2 and 7**).
4. Prior to the commencement of works, final design plans/drawings for the new boat ramp facility, including details of the external colours, finishes and materials, shall be submitted to, and approved by the Department of Biodiversity, Conservation and Attractions on advice from the Department of Transport (see **Advice Note 3 and 7**).
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On completion of works

9. Upon completion of the works, all waste materials, equipment and machinery shall be removed and the site cleaned-up to the satisfaction of the Department of Biodiversity, Conservation and Attractions.

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 - b. site access and management, including fencing requirements;
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 - e. protection of the river from inputs of debris, litter, rubbish or other deleterious material;
 - f. protection of vegetation (including tree roots);
 - g. sediment and turbidity management;
 - h. fauna management;
 - i. marine safety;
 - j. hours of operation and schedule of works; and
 - k. complaints and incidents.

The plan should address the most appropriate means of sediment and turbidity management for the proposed works. This may include the use of a silt curtain. All reasonable efforts should be taken to control plumes but where they occur, work must stop whilst the plume is addressed, and work should only recommence once the integrity of the device is restored. In the event that plumes, or adverse turbidity occur beyond the area of work, the applicant shall contact the Department of Biodiversity, Conservation and Attractions, Rivers and Estuaries Division within 2 hours on 9278 0900 or a/h 0419 192 845. Emergency spill kits should be onsite at all times and refuelling of machinery should be undertaken outside the Swan Canning Development Control Area. In case of

pollution events the Department of Biodiversity, Conservation and Attractions should be contacted immediately on 9278 0900 or a/h 0419 192 845. A site map showing the laydown area and vehicle entry/exit points should be included. Sand, soil, and any other finely grained material that is exposed or stockpiled on site shall be managed to prevent sediment mobilisation and discharge into the Canning River. The existing mature tree (including its roots) in close proximity to the scour protection should be appropriately protected during the construction works. Piling operations shall include a soft start up procedure at the commencement of each piling sequence. The contractor shall keep a constant watch for dolphins during piling operations. No pile driving shall start if dolphins are within 200m of the site and piling activity shall cease if a dolphin comes within 50m of the site and shall not recommence until any dolphin has moved away more than 200m or has not been observed for 20 minutes.

3. In relation to **Condition 4**, the Department of Transport advises that all detailed drawings/plans for the new boat ramp are to be signed and certified by an experienced maritime engineer to confirm that it has been designed in accordance with AS3962-2001 Guidelines for Design of Marinas and AS4997-2005 Guidelines for the Design of Maritime Structures. Any foreshore re-grading works adjacent to the scour protection should also be included on the plans. The selection of colours, finishes and materials should complement the character and landscape setting of the Canning River. In addition, given the river setting, the height of the navigational light should be reduced.
4. In relation to **Condition 5**, the *Acid Sulphate Soils Guideline Series for guidance on the identification, assessment and management of acid sulphate soils in Western Australia* is available from the Department of Water and Environmental Regulation website at www.dwer.wa.gov.au. If any ASS is exposed during the works, the Contaminated Sites Branch of the Department of Water and Environmental Regulation should be contacted for further advice on 1300 762 982. Any dewatering requirements will need to be addressed in the plan.
5. In relation to **Condition 6**, public access along the foreshore should be maintained, or an alternative route provided with appropriate signage, for the entirety of the works to the satisfaction of the Department of Biodiversity, Conservation and Attractions.
6. In relation to **Condition 7**, planting behind the scour protection is required to assist with improving its visual impact. The Landscaping Management Plan should include a list of plant species to be used, their location and densities and implantation schedule. Please note that all plant species are to be locally native and suited to the soil type of the area to sustain local biodiversity and due to their low fertiliser and water requirements. Details of any temporary fencing/screening to be implemented should also be provided.
7. To allow sufficient time for the Department of Biodiversity, Conservation and Attractions to consider and approve the plan and details required under **Conditions 3-7** inclusive, the documents should be submitted to the Department of Biodiversity, Conservation and Attractions no later than 30 days prior to the expected commencement date for the works.
8. The City of Melville will need to apply for, and obtain a jetty modification licence from the Department of Transport Coastal Facilities Management prior to the commencement of works.

9. In regard to marine safety, the applicant is advised to ensure that:
 - a. all in-water operations and activities must be conducted in accordance with Western Australian Marine Safety Legislation; and
 - b. all contractor vessels must display signs and shapes in accordance with the requirements of the *Prevention of Collision at Sea Regulations 1983*.
10. The Department of Transport (DoT) advises that the City of Melville, or any third party contractor it engages to carry out the works, notify DoT, Navigational Safety by email to navigational.safety@transport.wa.gov.au prior to the commencement of works to enable a Temporary Notice to Mariners outlining the scope of the works, locations, dates and navigational markings, to be issued, funded by the City of Melville or its contractor. It is advised that at least 21 days notice be provided.
11. The applicant is advised that the proposal has the potential to be affected by the 1% Annual Exceedance Probability Flood Level.
12. The applicant is advised to contact the Department of Planning, Lands and Heritage to ensure that the proposed works do not breach any section of Part IV (Protection of Aboriginal Sites) of the *Aboriginal Heritage Act 1972* (the AHA). Approval under section 18 of the AHA may be required. It is recommended that the applicant consult the recognised knowledge holders (see below) for Aboriginal Site 3536 on whether the works will impact the Canning River. Please refer to the Aboriginal Heritage Due Diligence Guidelines when planning specific development within a Heritage area.
13. A future interpretation node has been identified in the *Marli Riverpark: An Interpretation Plan for the Swan and Canning Riverpark* (Swan River Trust, April 2014) at Deep Water Point. The City of Melville is encouraged to work with the Department of Biodiversity, Conservation and Attractions to develop an interpretation node for this area.

FINAL REPORT ENDORSED

Signed:  Date: 22/2/18

Mark Webb
Director General

Attachment 1

Legend

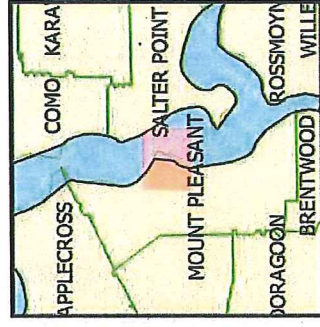
Swan Canning Development Control Area
Perth Central Cadastre (Land Parcels)




1:2,500 (A4)
0 25 50 75 m



Geographic Projection
Datum: GDA94



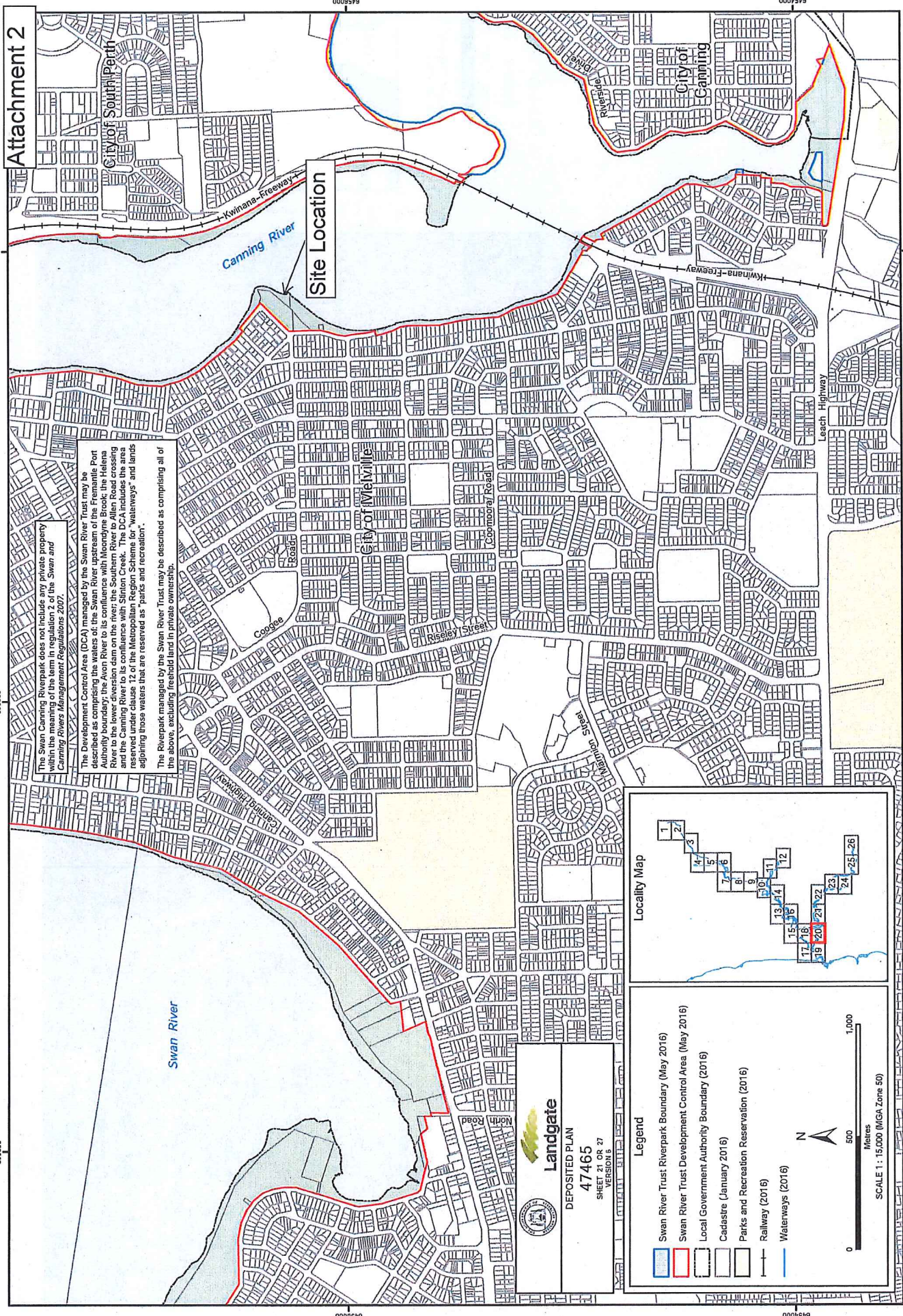
Produced by DBCA,
DBCA



Job Ref: 2017/4110
Produced at 12:01 PM on November 28, 2017



The DBCA does not guarantee that this map is without flaw of any kind and disclaims all liability for any errors, loss or other consequences which may arise from relying on any information depicted. Roads and tracks on land managed by DBCA may contain unmarked hazards and their surface condition is variable. Exercise caution and drive to conditions on all roads.



Attachment 2

The Swan Canning Riverpark does not include any private property within the meaning of the term in regulation 2 of the *Swan and Canning Rivers Management Regulations 2007*.

The Development Control Area (DCA) managed by the Swan River Trust may be described as comprising the waters of: the Swan River upstream of the Fremantle Port Authority boundary; the Avon River to its confluence with Moondyne Brook; the Helena River to the lower diversion dam on the river; the Southern River to Allen Road crossing and the Canning River to its confluence with the Swan River. The DCA includes the area between the Swan River and the Southern River, the Swan River, the waterways and lands adjoining those waters that are reserved as 'parks and recreation'.

The Riverpark managed by the Swan River Trust may be described as comprising all of the above, excluding freehold land in private ownership.

Landgate

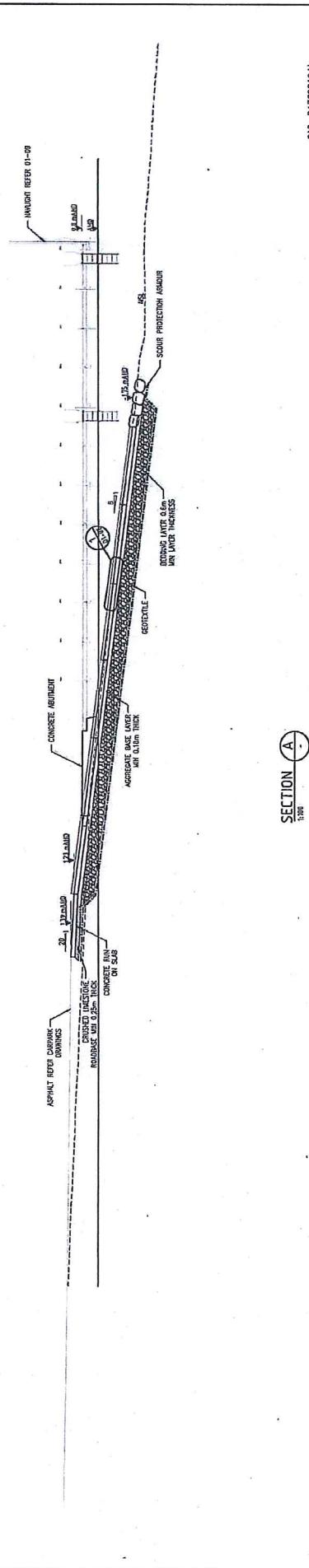
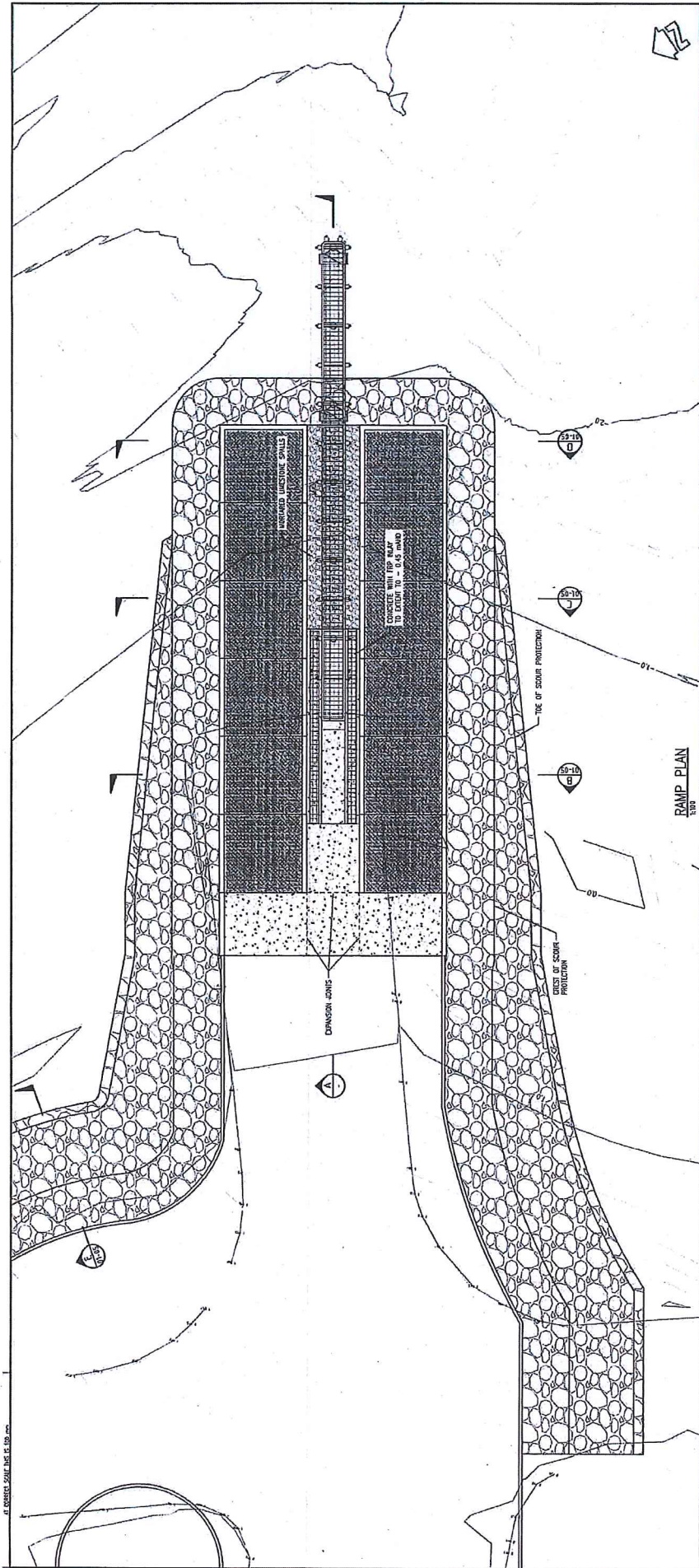
DEPOSITED PLAN
47465
SHEET 11 OF 27
VERSION 1.0

Legend

- Swan River Trust Riverpark Boundary (May 2016)
- Swan River Trust Development Control Area (May 2016)
- Local Government Authority Boundary (2016)
- Cadastre (January 2016)
- Parks and Recreation Reservation (2016)
- Railway (2016)
- Waterways (2016)

Locality Map

0 500 1,000
Metres
SCALE 1 : 15,000 (MGA Zone 50)



SECTION A
1:20

PROJECT: DEEPWATER POINT BOAT LAUNCHING FACILITY		DRAWING NUMBER: D1369-01-04	
TITLE: RAMP LAYOUT & TYPICAL SECTION		SCALE: A1:1	
DATE: 11/03/11		DATE: 11/03/11	
DRAWN BY: P. DOUST		CHECKED BY: P. DOUST	
DESIGNED BY: T. HARDING		APPROVED BY: P. DOUST	
PROJECT: CITY OF MELVILLE		PROJECT: CITY OF MELVILLE	
CONTRACTOR: m p rogers & associates pl		CONTRACTOR: m p rogers & associates pl	
ADDRESS: 103 Mac Street, Subiaco WA 6107		ADDRESS: 103 Mac Street, Subiaco WA 6107	
PHONE: 08 9 424 8888		PHONE: 08 9 424 8888	
FAX: 08 9 424 8889		FAX: 08 9 424 8889	
EMAIL: info@mprogers.com.au		EMAIL: info@mprogers.com.au	
WEBSITE: www.mprogers.com.au		WEBSITE: www.mprogers.com.au	
ISSUED FOR APPROVALS		ISSUED FOR APPROVALS	
NO.	DATE	BY	CHK
1	11/03/11	P. DOUST	P. DOUST

CAD: D13690104A

m p rogers & associates pl
coastal and port engineers
103 Mac Street
Subiaco WA 6107
Phone: 08 9 424 8888
Fax: 08 9 424 8889
Email: info@mprogers.com.au
Website: www.mprogers.com.au

This plan is not to be used unless stated as being issued for approval.

CONTRACTOR: m p rogers & associates pl
103 Mac Street, Subiaco WA 6107
Phone: 08 9 424 8888
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Email: info@mprogers.com.au
Website: www.mprogers.com.au

ISSUED FOR APPROVALS

NO. DATE BY CHK

1 11/03/11 P. DOUST P. DOUST

ISSUED FOR APPROVALS

NO. DATE BY CHK

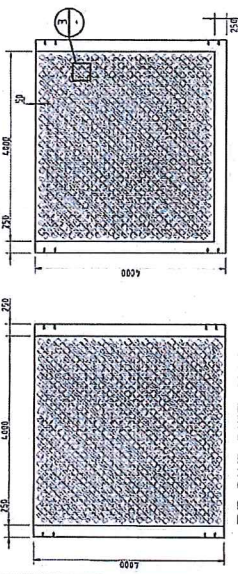
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ISSUED FOR APPROVALS

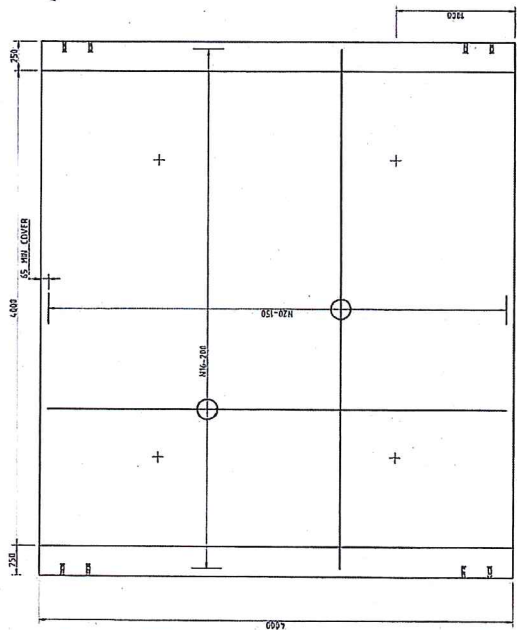
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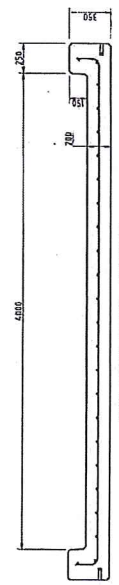
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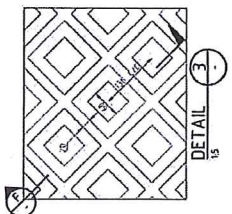
TYP RAMP PANEL PLAN
1:250



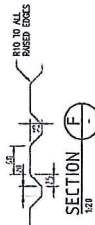
TYP RAMP PANEL REINFORCEMENT PLAN
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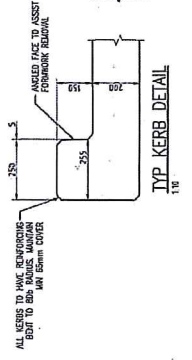
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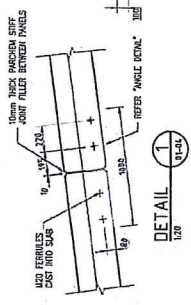
DETAIL 3
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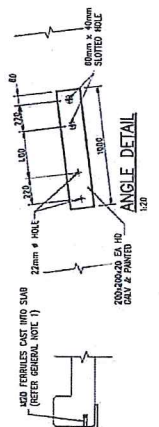
SECTION F
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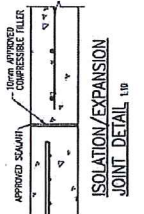
TYP KERB DETAIL
1:20



DETAIL 1
1:20

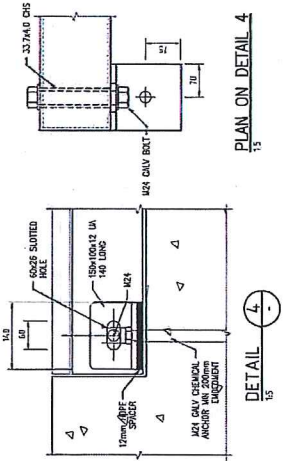
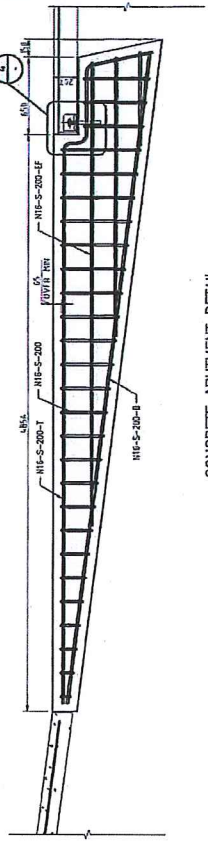


ANGLE DETAIL
1:20



ISOLATION/EXPANSION JOINT DETAIL
1:20

CONCRETE ABUTMENT DETAIL
1:20



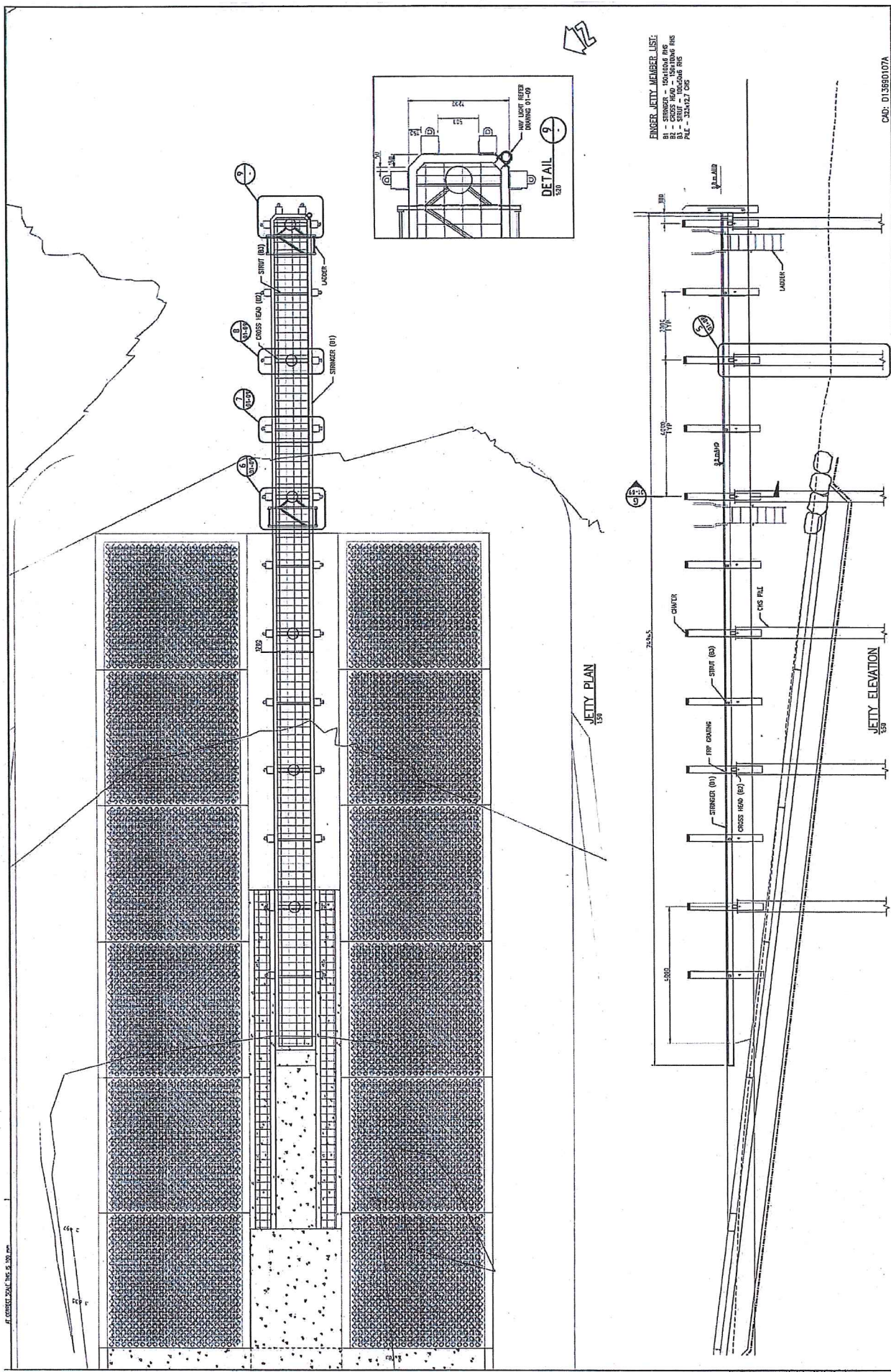
DETAIL 4
1:20

- CONCRETE NOTES:**
1. ALL FERRELS & ANCHORS TO BE INSTALLED AT ALL PERIMETER JOINTS & ANGLES TO THE SATISFACTION OF THE CITY'S REPRESENTATIVE.
 2. CONCRETE SHALL CONFORM WITH THE REQUIREMENTS OF THE SPECIFICATION AND AS 1379:2007.
 3. MECHANICALLY VIBRATE CONCRETE IN THE FORM TO OBTAIN MAXIMUM COMPACTION WITHOUT SEGREGATION OF THE CONCRETE.
 4. CONCRETE SHALL BE WATER CURED FOR A MINIMUM PERIOD OF 7 DAYS AS REQUIRED BY THE SPECIFICATION.
 5. DO NOT MAKE UNNECESSARY HOLES OR CHISELS WITHOUT DESIGN ENGINEER'S APPROVAL.

REINFORCEMENT NOTES:

1. ALL REINFORCEMENT TO BE GALVANIZED.
2. REINFORCING BARS TO BE:
 - > 16mm BAR - R20
 - > 16mm BAR - R26
3. BAR MOTION CUES THE FOLLOWING INFORMATION IN THIS ORDER: NO. OF BARS; GRID BAR SIZE (mm); SPACING (mm); PLACING APPROVAL.
4. ALL REINFORCEMENT NOT ROLLED DEFORMED REINFORCING BAR TO ADVISE APPROVAL.
5. REINFORCEMENT IS REPRESENTED DIMENSIONALLY AND NOT NECESSARILY IN TRUE POSITION.
6. REINFORCEMENT DIMENSIONS SHALL NOT BE SCALED.
7. WHERE THE REINFORCEMENT DIMENSIONS DO NOT MATCH THE DIMENSIONS OF THE FORMWORK, THE DIMENSIONS OF THE FORMWORK SHALL TAKE PRECEDENCE OVER THE DIMENSIONS OF THE REINFORCEMENT.
8. THE COVER TO THE REINFORCEMENT NEAREST CONCRETE SURFACE SHALL NOT BE LESS THAN 65mm EXCEPT WHERE SPECIFIED OTHERWISE.

PROJECT: DEEPWATER POINT BOAT LAUNCHING FACILITY		CAD: D13690106A	
CONCRETE DETAILS		DRAWN: AS SHOWN	
DATE: 11/11/2011		REV: A	
<p>m p rogers & associates pl coastal and port engineers Suite 4, 158 York Street Osborne Park, WA 6107 Western Australia t: 08 9254 8500 f: 08 9254 8509 e: info@mpregers.com.au</p>		<p>CITY OF MELVILLE T. HARDING U. VAN BEEM</p>	
APPROVED	PHD	APPROVED	MANAGER
ISSUED FOR APPROVAL	PHD	APPROVED	MANAGER
DATE	11/11/2011	DATE	11/11/2011



FINGER JETTY MEMBER LIST:
 B1 - STRIKER - 10000 LB
 B2 - CROSS HEAD - 10000 LB
 B3 - STRIKER - 10000 LB
 B4 - STRIKER - 10000 LB
 B5 - STRIKER - 10000 LB
 B6 - STRIKER - 10000 LB
 B7 - STRIKER - 10000 LB
 B8 - STRIKER - 10000 LB
 B9 - STRIKER - 10000 LB
 B10 - STRIKER - 10000 LB
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 B100 - STRIKER - 10000 LB

PROJECT: DEEPWATER POINT BOAT LAUNCHING FACILITY CAD: D13690107A	
TITLE: FINGER JETTY LAYOUT & TYPICAL SECTION	
DESIGNER: m p rogers & associates pl coastal and port engineers 1015 17th Street Westport, Arkansas Phone: 501-825-5559 Fax: 501-825-5559 Email: info@mpce.com	DRAWN BY: AS SHOWN CHECKED BY: AS SHOWN DATE: D1369-01-07 SHEET: 1 OF 1
CITY OF MELVILLE ENGINEER: T. VAN BEEM ARCHITECT: P. DOUST	APPROVED: P. DOUST APPROVED: P. DOUST
COMMENTS: This plan is not to be used for construction without the approval of the City of Melville.	APPROVED: P. DOUST
APPROVED: P. DOUST	APPROVED: P. DOUST

